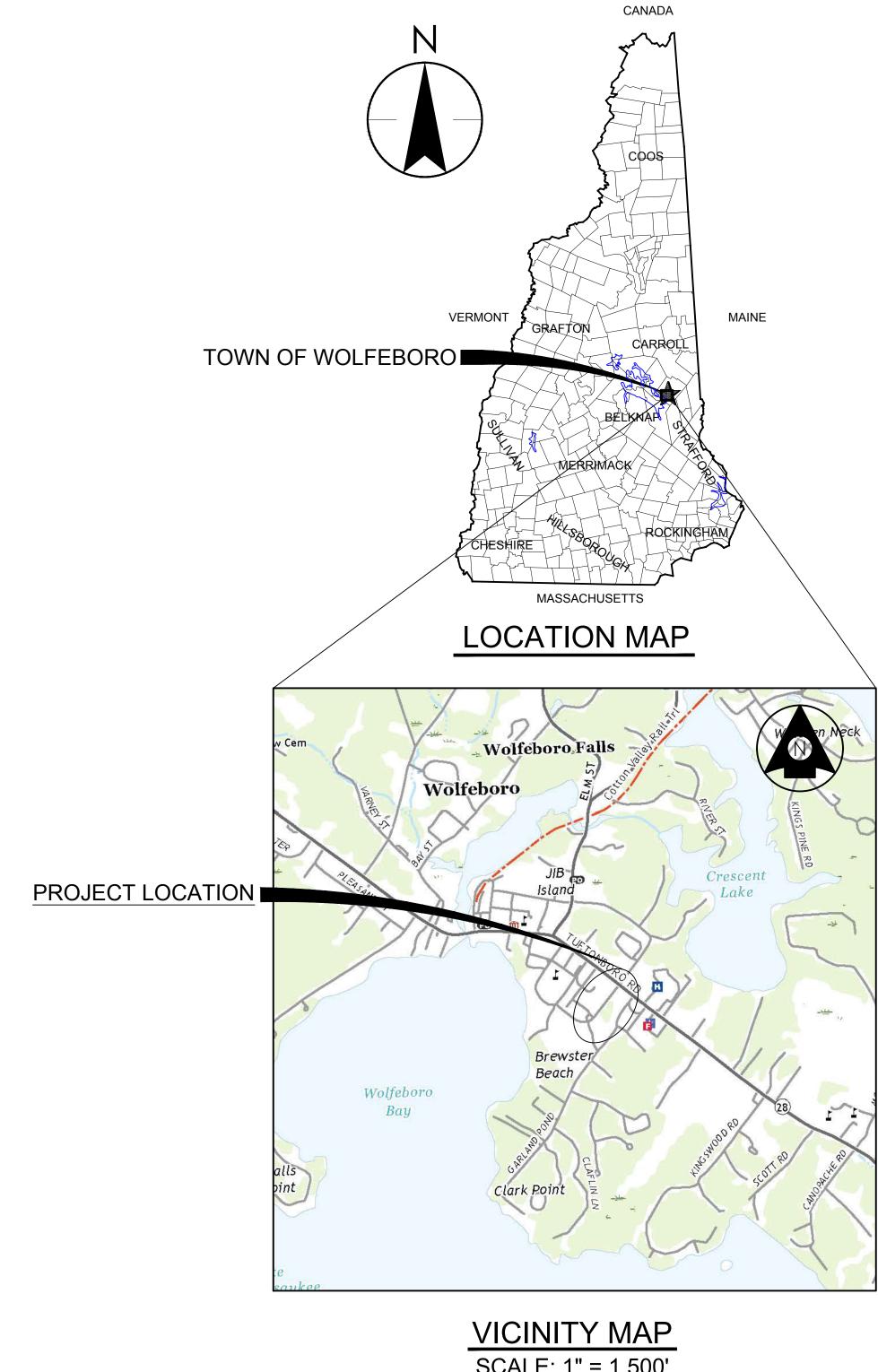


TOWN OF WOLFEBORO, NH

RECONSTRUCTION PROJECT



SCALE: 1" = 1,500'

MARCH 2023 PROJECT NUMBER: 179450615

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GENERAL NOTES

- THE MINIMUM ACCEPTABLE STANDARDS FOR ALL CONSTRUCTION MATERIALS AND METHODS SHALL BE IN ACCORDANCE WITH THE NEW HAMPSHIRE DEPARTMENT OF TRANSPORTATION (NHDOT) STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, 2016 EDITION AND ALL SUBSEQUENT AMENDMENTS. ITEM NUMBERS REFERENCED IN THE DRAWINGS AND THE CONTRACT DOCUMENTS ARE NHDOT ITEM NUMBERS.
- 2. CONTRACTOR SHALL NOTIFY DIG-SAFE (1-888-344-7233) A MINIMUM OF 72 HOURS PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL REPAIR, AT HIS/HER OWN EXPENSE, ANY UNDERGROUND CABLES OR UTILITIES DAMAGED BY HIS/HER OPERATIONS INCLUDING ANY DAMAGE DONE DRIVING HIS/HER EQUIPMENT OVER EXISTING UNDERGROUND CABLES OR UTILITIES.
- 3. ALL TREE TRIMMING MUST BE REVIEWED BY THE TOWN OF WOLFEBORO AND ENGINEER PRIOR TO BEGINNING ANY CLEARING OPERATIONS.
- 4. SIGN NOTES ARE SHOWN ON THE CONSTRUCTION DETAIL SHEETS.
- GREEN STREET SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES, EXCEPT WHEN A TEMPORARY DETOUR SHALL BE USED. NO DETOUR SHALL BE USED WITHOUT PRIOR APPROVAL OF THE ENGINEER. WHEN A DETOUR IS NOT IN PLACE, ONE WAY TRAFFIC MUST BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. TWO WAY TRAFFIC MUST BE RESTORED AT THE END OF EACH WORK DAY. THE CONTRACTOR MUST CONTACT THE WOLFEBORO FIRE, POLICE AND SCHOOL DEPARTMENTS PRIOR TO BEGINNING CONSTRUCTION AT THE START OF EACH WORK DAY. THE CONTRACTOR SHALL PROVIDE ALL SIGNAGE, BARRICADES AS REQUIRED FOR TRAFFIC CONTROL AND COORDINATION WITH OTHER CONSTRUCTION OPERATIONS ON ADJACENT STREETS. ALL TRAFFIC CONTROL PLANS SHALL BE SUBMITTED FOR REVIEW AND APPROVAL BY THE TOWN AT LEAST 7 DAYS PRIOR TO CONSTRUCTION.
- 6. THE CONTRACTOR SHALL COORDINATE ALL CONSTRUCTION OPERATIONS AND DETOURS WITH OTHER TOWN PROJECTS.
- 7. WORK SHALL NOT BEGIN BEFORE 7:00 AM AND MUST END BY 5:00 PM UNLESS OTHERWISE APPROVED BY THE TOWN IN WRITING.
- 8. ALL PERMANENT CONSTRUCTION WARNING SIGNS MUST BE ERECTED PRIOR TO BEGINNING CONSTRUCTION.
- ALL PERMANENT AND TEMPORARY TRAFFIC CONTROL SIGNS SHALL MEET THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
 - A. PERMANENT CONSTRUCTION SIGNS SHALL CONFORM TO NHDOT "WORK ZONE TRAFFIC CONTROL STANDARD PLANS" SHEET TC-1, TC-2, TC-3 AND TC-8 WITH REGARD TO SIGN TYPE, QUANTITY AND LAYOUT.
 - TEMPORARY CONSTRUCTION SIGNS USED FOR DAILY LANE CLOSURES AND TRAFFIC CONTROL SHALL CONFORM TO NHDOT "WORK ZONE TRAFFIC CONTROL STANDARD PLANS" SHEET TC-2, TC-3 AND TC-8 WITH REGARD TO TAPER LENGTHS, CHANNELIZING DEVICES AND SIGN TYPE, QUANTITY AND LAYOUT. REMOVE THOSE EXCESS MATERIALS FROM SITE AT NO EXPENSE TO THE OWNER.
- 10. THE CONTRACTOR MUST COORDINATE ALL WORK WITH THE TOWN OF WOLFEBORO, ENGINEER AND ANY AFFECTED ABUTTERS. ALL WORK OUTSIDE THE ROADWAY ROW SHALL BE COORDINATED WITH THE AFFECTED PROPERTY OWNER. WORK SHALL NOT PROCEED WITHOUT APPROVAL OF THE WOLFEBORO ENGINEERING DEPARTMENT.
- 11. ALL ELEVATIONS AND LOCATIONS OF EXISTING UTILITY AND DRAINAGE STRUCTURES SHALL BE VERIFIED IN THE FIELD BY THE CONTRACTOR PRIOR TO UTILIZATION OF THE PROPOSED DESIGNS SHOWN ON THE PLAN.
- 12. BACKFILL OF THE TRENCHES SHALL BE COMPACTED TO TO 95% MAX. DRY DENSITY UNDER ALL PAVED AREAS AND 92% MAX. DRY DENSITY UNDER OTHER AREAS IN ACCORDANCE WITH NHDOT STANDARD SPECIFICATIONS - SECTION 304.

→ EXISTING SIGN

----- EXISTING EDGE OF PAVEMENT

EXISTING WATER MAIN

LEGEND

- PROPOSED DROP INLET

(III) PROPOSED DROP INLET TO EXISTING HYDRANT EXISTING RIGHT-OF-WAY PROPOSED TEST PIT — 100 — EXISTING MAJOR CONTOUR PROPOSED DITCH/SWALE — 98 — EXISTING MINOR CONTOUR EXISTING STONE WALL ------ SD ------ PROPOSED STORM DRAIN ---- EXISTING WOODEN FENCE EXISTING GUARD RAIL EXISTING STORM DRAIN ——— — EXISTING STREAM

- 13. SHORING AND STABILIZATION OF TRENCH SIDEWALLS DURING EXCAVATION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- 14. ALL CULVERTS, DRAINAGE STRUCTURES, UTILITY WORK, AND ROAD CONSTRUCTION SHALL BE SUBJECT TO FINAL INSPECTION BY THE WOLFEBORO ENGINEERING DEPARTMENT PRIOR TO ACCEPTANCE BY THE TOWN OF WOLFEBORO.
- 15. THE CONTRACTOR SHALL PROVIDE A MINIMUM OF 4" TOPSOIL AND SEED OVER ALL DISTURBED UNPAVED AREAS UNLESS OTHERWISE SPECIFIED.
- 16. ALL PAVEMENT CUTTING SHALL BE COMPLETED BY SAW CUTTING ONLY.
- 17. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL DIMENSIONS AND FITTING OF VARIOUS ITEMS OF WORK AT THE PROJECT SITE.
- 18. THE CONTRACTOR SHALL SUBMIT HIS/HER PROPOSED CONSTRUCTION SCHEDULE TO THE WOLFEBORO ENGINEERING DEPARTMENT FOR REVIEW AND APPROVAL AT LEAST TWO (2) WEEKS PRIOR TO THE START OF CONSTRUCTION. NO WORK SHALL BE CONDUCTED WITHOUT AN APPROVED SCHEDULE. UPDATED SCHEDULES SHALL BE PROVIDED TO THE WOLFEBORO ENGINEERING DEPARTMENT FOR REVIEW AND APPROVAL EVERY TWO (2) WEEKS DURING CONSTRUCTION.
- 19. BLASTING, IF REQUIRED, SHALL BE PERFORMED BY A LICENSED BLASTING CONTRACTOR MEETING NHDOT AND TOWN OF WOLFEBORO REQUIREMENTS.
- 20. THE CONTRACTOR SHALL BE RESPONSIBLE TO COORDINATE WITH THE WOLFEBORO MUNICIPAL ELECTRIC DEPT., METROCAST, FAIRPOINT COMMUNICATIONS, COMCAST AND ALL OTHER AFFECTED UTILITY COMPANIES.
- 21. THE CONTRACTOR SHALL MEET THE REQUIREMENTS OF THE EPA NPDES PHASE II PROGRAM AND SUBSEQUENT PROGRAMS AS APPLICABLE AT THE TIME OF CONSTRUCTION OF THIS PROJECT.
- 22. THE CONTRACTOR SHALL PROVIDE FOR AND MAINTAIN CONTINUOUS DRAINAGE THROUGHOUT CONSTRUCTION.
- 23. ALL EXCESS MATERIALS ON SITE SHALL BE OFFERED FIRST TO THE OWNER AND TRANSPORTED TO A LOCATION SPECIFIED BY THE OWNER AT NO ADDITIONAL CHARGE. SHOULD THE OWNER NOT WANT SOME OR ALL THE MATERIALS, THE CONTRACTOR SHALL REMOVE THOSE EXCESS MATERIALS FROM SITE AT NO EXPENSE TO THE TOWN OF WOLFEBORO.
- 24. THE CONTRACTOR SHALL TAKE ALL NECESSARY MEASURES AND SHALL PROVIDE ALL NECESSARY CONTINUOUS BARRIERS OF SUFFICIENT TYPE, SIZE AND STRENGTH TO PREVENT ACCESS TO ALL OPEN EXCAVATIONS AT THE COMPLETION OF EACH DAYS WORK.
- 25. WOOD FROM TREES TO BE REMOVED FROM THE PROJECT SHALL BE FIRST OFFERED TO THE ADJACENT PROPERTY OWNER. SHOULD THE ADJACENT PROPERTY OWNER NOT WANT SOME OR ALL THE WOOD, THE CONTRACTOR SHALL REMOVE THE EXCESS WOOD FROM SITE AT NO EXPENSE TO THE TOWN OF WOLFEBORO.
- 26. STUMP REMOVAL WILL BE PAID UNDER ITEM NO. 201.4.
- 27. CONTRACTOR TO PERFORM TEST PITS PRIOR TO ORDERING STRUCTURES TO VERIFY ELEVATIONS AND REQUIRED OPENINGS.
- 28. CATCH BASIN RIM ELEVATIONS SHALL BE SET TO THE PROPER ELEVATIONS WITHIN THE SHOULDERS TO PROVIDE FOR CONTINUOUS DRAINAGE, (NO PONDING). RIM ELEVATIONS ARE NOT PROVIDED IN THIS PLAN SET.
- 29. ALL WORK IN AND ADJACENT TO TOWN STREETS SHALL BE PERFORMED IN ACCORDANCE WITH THE REQUIREMENTS OF THE TOWN OF WOLFEBORO STREET OPENING PERMIT AND NHDOT STANDARDS AND SPECIFICATIONS. THE CONTRACTOR IS REQUIRED TO OBTAIN ALL THE NECESSARY PERMITS FROM THE TOWN OF WOLFEBORO AND NHDOT — DISTRICT 3 PRIOR TO THE START OF CONSTRUCTION.
- 30. NO FIELD SURVEY HAS BEEN PERFORMED FOR THIS PROJECT. THE CONTRACTOR SHALL PROTECT AND MAINTAIN ALL EXISTING MONUMENTS. MONUMENTS WHICH ARE TO BE REMOVED AND RESET SHALL BE RESET TO THE EXISTING LOCATION BY A LICENSED LAND SURVEYOR, RESET MONUMENTS WILL BE PAID UNDER ITEM NO. 622.55.
- 31. IF, DURING CONSTRUCTION, IT BECOMES APPARENT THAT ADDITIONAL EROSION CONTROL MEASURES ARE REQUIRED TO STOP ANY EROSION ON THE CONSTRUCTION SITE DUE TO ACTUAL SITE CONDITIONS, OR AT THE REQUEST OF THE OWNER, THE CONTRACTOR SHALL BE REQUIRED TO INSTALL THE NECESSARY EROSION PROTECTION AT NO EXPENSE TO THE OWNER.
- 32. THE CONTRACTOR SHALL COMPLY WITH THE CONDITIONS OF THE PROJECT PERMITS (SEE CONTRACT DOCUMENTS). THIS WORK SHALL BE CONSIDERED TO BE INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION SHALL BE ALLOWED.
- 33. THE CONTRACTOR MUST ACQUIRE ALL PERMITS WITH NHDOT DISTRICT 3, FOR THE PROPOSED WORK IN THE NHDOT RIGHT OF WAY. THE PROPOSED WATER MAIN WORK WITHIN THE NHDOT RIGHT OF WAY MUST TAKE PLACE AT NIGHT.
- 34. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SNOW REMOVAL REQUIRED FOR CONSTRUCTION. PLACING AND REMOVING SNOW ON PRIVATE PROPERTY SHALL NOT BE ALLOWED. THE REMOVAL OF SNOW SHALL BE CONSIDERED INCIDENTAL TO CONSTRUCTION AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- 35. ALL TESTING WILL BE THE RESPONSIBILITY OF THE CONTRACTOR
- 36. ALL WORK SHALL TAKE PLACE WITHIN THE BOUNDARIES OF THE EXISTING RIGHT OF WAY. THE PROPERTY LINES SHOWN ON THE PLANS ARE APPROXIMATE. THE EXISTING STONE WALLS THROUGHOUT THE PROJECT ARE CONSIDERED TO BE RIGHT OF WAY BOUNDARIES. THE CONTRACTOR SHALL NOTIFY THE TOWN AND THE TOWN'S ENGINEER PRIOR ANY WORK THAT MAY IMPACT THE STONE WALLS. THE CONTRACTOR MUST TAKE BEFORE AND AFTER PHOTOS OF ANY WALLS THAT ARE IMPACTED DURING CONSTRUCTION. A PRECONSTRUCTION VIDEO IS REQUIRED FOR THE ENTIRE LENGTH OF THIS PROJECT.
- 37. THE NHDOT 304.323 CRUSHED GRAVEL FOR SHOULDERS SHALL BE MIXED WITH 30% LOAM.
- 38. NO TEST PITS WERE PERFORMED FOR THIS PROJECT.
- 39. EXISTING DRAINAGE CULVERTS/PIPES ARE INTENDED TO BE RETAINED AT THE SAME LOCATION AND ELEVATION. THE CONTRACTOR MUST COORDINATE ALL CB/DMH ELEVATIONS WITH THE ENGINEER AND THE TOWN PRIOR TO FINAL ADJUSTMENT.
- 40. THE PROPOSED WATER MAIN IS INTENDED TO BE INSTALLED IN THE SAME TRENCH AS THE EXISTING WATER MAIN WITH 5.5' OF COVER. NO ADDITIONAL PAYMENT WILL BE MADE FOR THE REMOVAL OF THE EXISTING WATER PIPE WITHIN THE SAME TRENCH AS THE NEW PIPE. ANY PIPE REMOVAL OUTSIDE OF THE PROPOSED TRENCH WILL BE PAID FOR UNDER ITEM NO. 202.41.
- 41. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE INSTALLATION AND CONNECTION OF THE TEMPORARY WATER MAIN, ITEM NO. 611.992. THE CONTRACTOR MUST COORDINATE THE INSTALLATION OF THE TEMPORARY WATER MAIN WITH THE TOWN OF WOLFEBORO WATER DEPARTMENT.
- 42. NEW DRIVEWAY APRONS ARE INTENDED TO BE SAWCUT AND REPAVED, APPROXIMATELY 5' FROM THE EDGE OF ROADWAY PAVEMENT. SEE RESIDENTIAL DRIVEWAY SECTION ON DETAIL SHEET C-004.



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Consultant

Notes

_____ AJG AJG JRB March 2023

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By Appd YYYY.MM.DD

Permit/Seal

Client/Project Logo

Client/Project TOWN OF WOLFEBORO

GREEN ST ROADWAY RECONSTRUCTION PROJECT

Wolfeboro, New Hampshire

INDEX AND GENERAL NOTES

Project No. 179450615 Revision Sheet

Scale AS NOTED Drawing No.

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ORIGINAL SHEET - ARCH D

EARTHWORK

- SECTION 202 PARTS 1, 3.3 AND 3.7 REMOVAL OF STRUCTURES AND OBSTRUCTIONS, SECTION 203 - PARTS 1, 2 AND 3 - EXCAVATION AND EMBANKMENT, SECTION 206 - PARTS 1, 2 AND 3 - STRUCTURE EXCAVATION FOR PIPES AND OTHER MINOR STRUCTURES, AND SECTION-304 - PARTS 1, 2 AND 3-AGGREGATE BASE COURSE OF THE NHDOT SPECIFICATIONS SHALL ALSO APPLY TO THIS SECTION.
- 2. EXCAVATION CONSISTS OF REMOVAL OF MATERIAL ENCOUNTERED TO SUBGRADE ELEVATIONS INDICATED AND SUBSEQUENT DISPOSAL OF MATERIALS REMOVED. UNAUTHORIZED EXCAVATION CONSISTS OF REMOVAL OF MATERIALS BEYOND INDICATED SUBGRADE ELEVATIONS OR DIMENSIONS WITHOUT SPECIFIC DIRECTION OF THE ENGINEER. UNAUTHORIZED EXCAVATION, AS WELL AS REMEDIAL WORK FOR CORRECTION, SHALL BE AT THE CONTRACTOR'S EXPENSE.
- 3. SUBGRADE: THE UNDISTURBED EARTH OR THE COMPACTED SOIL LAYER IMMEDIATELY BELOW GRANULAR SUBBASE.
- STRUCTURE: BUILDINGS, FOUNDATIONS, SLABS, TANKS, CURBS, OR OTHER MAN-MADE STATIONARY FEATURES OCCURRING ABOVE OR BELOW GROUND
- 5. FINISHED GRADE: THE WORD "FINISHED GRADE" AS USED HEREIN SHALL MEAN THE REQUIRED FINAL GRADE ELEVATIONS INDICATED ON THE CONTRACT DRAWINGS. SPOT ELEVATIONS SHALL GOVERN OVER PROPOSED CONTOURS. WHERE NOT OTHERWISE INDICATED, PROJECT SITE AREAS SHALL BE GIVEN UNIFORM SLOPES BETWEEN POINTS FOR WHICH FINISHED GRADES ARE INDICATED OR BETWEEN SUCH POINTS AND EXISTING ESTABLISHED GRADES.
- 6. NOTIFICATION FOR APPROVAL OF FILL MATERIAL, THE CONTRACTOR
- A. NOTIFY THE ENGINEER AT LEAST 14 WORKING DAYS IN ADVANCE OF INTENTION TO IMPORT MATERIAL.
- B. SUBMIT A CERTIFICATE OF COMPLIANCE TO THE ENGINEER FOR EACH MATERIAL. CERTIFICATE SHALL INCLUDE:
- THE CONTRACTOR'S STATEMENT THAT THE NAMED MATERIAL MEETS
- THE SPECIFIED REQUIREMENTS. A COPY OF THE TESTING LABORATORY'S REPORT.
- THE LOCATION OF THE MATERIAL'S ORIGIN WITH DATE OF TEST ANALYSIS HIGHLIGHTED.
- SOURCES SHALL BE ACCESSIBLE TO THE OWNER, OR HIS AGENT, FOR INSPECTION OR ADDITIONAL SAMPLING AND TESTING.
- THE REQUIREMENTS OF THE 2018 INTERNATIONAL BUILDING CODE AND THE APPLICABLE PORTIONS OF THE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGES, 2016, ESTABLISH THE MINIMUM ACCEPTABLE QUALITY OF WORKMANSHIP AND MATERIALS, AND ALL WORK SHALL CONFORM THERETO UNLESS MORE STRINGENT REQUIREMENTS ARE INDICATED OR SPECIFIED BY THE CONTRACT
- 8. DUST CONTROL USE ALL MEANS NECESSARY TO CONTROL DUST ON AND NEAR THE WORK AND ON AND NEAR ALL OFF-SITE BORROW AREAS IF SUCH DUST IS CAUSED BY THE CONTRACTOR'S OPERATIONS DURING PERFORMANCE OF THE WORK OR IF RESULTING FROM THE CONDITION IN WHICH THE CONTRACTOR LEAVES THE SITE. THOROUGHLY MOISTEN ALL SURFACES AS REQUIRED TO PREVENT DUST FROM BEING A NUISANCE TO THE OWNER, PUBLIC, AND CONCURRENT PERFORMANCE OF OTHER WORK ON THE SITE.
- 9. BRACING PROPERLY SUPPORT ALL TRENCHES AND OTHER EXCAVATIONS IN STRICT ACCORDANCE WITH ALL PERTINENT, CURRENT OSHA RULES AND REGULATIONS. BRACE, SHEET, AND SUPPORT TRENCH WALLS AND OTHER EXCAVATIONS IN SUCH A MANNER THAT THEY WILL BE SAFE AND THAT THE GROUND ALONGSIDE THE EXCAVATION WILL NOT SLIDE OR SETTLE, AND THAT ALL EXISTING IMPROVEMENTS OF EVERY KIND, WHETHER ON PUBLIC OR PRIVATE PROPERTY, WILL BE FULLY PROTECTED FROM DAMAGE. IN THE EVENT OF DAMAGE TO SUCH IMPROVEMENTS, IMMEDIATELY MAKE ALL REPAIRS AND REPLACEMENTS NECESSARY TO THE APPROVAL OF THE ENGINEER AND AT NO ADDITIONAL COST TO THE OWNER.
- 10. CRUSHED STONE FOR BEDDING SHALL BE DURABLE CRUSHED ROCK CONSISTING OF THE ANGULAR FRAGMENTS OBTAINED BY BREAKING AND CRUSHING SOLID OR SHATTERED NATURAL ROCK. AND REASONABLY FREE FROM SAND, CLAY, LOAM OR DELETERIOUS MATERIAL AND NOT MORE THAN 1% OF SATISFACTORY MATERIAL PASSING A NO. 200 SIEVE WILL BE ALLOWED TO ADHERE TO THE CRUSHED STONE. THE CRUSHED STONE SHALL BE UNIFORMLY BLENDED ACCORDING TO THE GRADING REQUIREMENTS FOR THE RESPECTIVE STONE SIZES SHOWN IN THE FOLLOWING TABLE FOR ASTM SPECIFICATION C33, STONE SIZE NUMBER 67 AS FOLLOWS:

SQUARE OPENING SIEVE	PERCENT BY WEIGH PASSING THROUGH
1"	100
3/4"	90 - 100
3/8"	20 - 55
NO. 4	0 - 10
NO. 8	0 - 5

11. SUBBASE MATERIAL UNDER PAVED SURFACES - SHALL CONSIST OF NATURALLY OR ARTIFICIALLY GRADED MIXTURE OF CRUSHED GRAVEL, AND NATURAL OR CRUSHED SAND AND SHALL CONFORM TO NHDOT SPECIFICATIONS - SECTION 304 - AGGREGATE BASE COURSE AS

CRUSHED STONE FINE: SECTION 304 ITEM 304.4 (TABLE 1)

- 12. FAMILIARIZATION PRIOR TO ALL WORK IN THIS SECTION, THE CONTRACTOR SHALL BECOME THOROUGHLY FAMILIAR WITH THE SITE, THE BUILDING AND SITE CONDITIONS, AND ALL PORTIONS OF THE WORK. THE CONTRACTOR SHALL SATISFY HIMSELF, BY AN ACTUAL SITE EXAMINATION OF THE WORK, AS TO THE EXISTING CONDITIONS, CONTOURS AND ELEVATIONS AND THE AMOUNT OF WORK REQUIRED UNDER THIS PROJECT.
- 13. PROVIDE AND MAINTAIN AT ALL TIMES DURING CONSTRUCTION, AMPLE MEANS AND DEVICES WITH WHICH TO REMOVE PROMPTLY AND DISPOSE OF ALL WATER FROM EVERY SOURCE ENTERING THE EXCAVATIONS OR OTHER PARTS OF THE WORK. DEWATER BY MEANS WHICH WILL ENSURE DRY EXCAVATIONS AND THE PRESERVATION OF THE FINAL LINES AND GRADES OF BOTTOMS OF EXCAVATIONS.

PAVEMENT

- BITUMINOUS CONCRETE SHALL BE PREPARED, MIXED, TRANSPORTED, PLACED, COMPACTED, AND FINISHED IN ACCORDANCE WITH THE REQUIREMENTS SET FORTH IN THE 2016 EDITION OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" PUBLISHED BY THE STATE OF NEW HAMPSHIRE, DEPARTMENT OF TRANSPORTATION. SPECIFICALLY, 401 PLANT MIX PAVEMENTS - GENERAL, SECTION 403 -HOT BITUMINOUS PAVEMENT, AND SECTION 410 - BITUMINOUS SURFACE TREATMENT OF THE NHDOT SPECIFICATIONS SHALL APPLY TO THIS
- 2. THE CONTRACTOR SHALL SUPPLY ALL MATERIALS, LABOR, SERVICES, EQUIPMENT, AND TOOLS REQUIRED TO PERFORM ALL HOT-MIXED ASPHALT PAVING AND RELATED WORK NECESSARY FOR PROPER COMPLETION OF THE WORK AS DEFINED BY THE CONTRACT DOCUMENTS.
- 3. THE CONTRACTOR SHALL REMOVE AND REPLACE ALL PAVEMENT DAMAGED IN ANY WAY BY HIS OPERATIONS ON ALL ROADS, STREETS, DRIVES, OR
- 4. ALL EXISTING PAVEMENT TO BE REMOVED SHALL BE SEVERED BY A SAWCUT. PAVEMENT SAW CUTS DAMAGED DURING THE COURSE OF THE WORK SHALL BE RESAWN, PROVIDING A PERPENDICULAR EDGE, JUST PRIOR TO INSTALLATION OF THE NEW PAVEMENT.
- 5. CERTIFICATES THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER, SWORN MATERIAL CERTIFICATES FROM THE SUPPLIERS OF BITUMINOUS CONCRETE MATERIALS. THE CERTIFICATES SHALL INDICATE THAT THE MATERIALS PROVIDED ARE IN EVERY WAY IN CONFORMANCE WITH THE REQUIREMENTS OF THESE SPECIFICATIONS.
- PRE-PAVING CONFERENCE: THE CONTRACTOR SHALL ARRANGE A PRE-PAVING CONFERENCE WITH THE ENGINEER TO REVIEW AND APPROVE THE PAVING SCHEDULE, PERSONNEL INVOLVED, THE SOURCE OF PAVING MIX AND THE TYPE OF PAVING MIX
- SETTLEMENT SHOULD ANY PAVEMENT SETTLEMENT OCCUR WITHIN ONE YEAR OF COMPLETION OF THE CONTRACT, THE CONTRACTOR SHALL REMOVE AND REPLACE SUCH PAVEMENT AT HIS OWN EXPENSE AS DIRECTED BY THE ENGINEER OR THE OWNER. IF THE CONTRACTOR FAILS TO MAKE SUCH REPAIRS PROMPTLY UPON RECEIPT OF NOTICE TO DO SO FROM THE OWNER. THEN THE OWNER MAY MAKE SUCH REPAIRS AS NECESSARY AND THE COSTS INCURRED IN MAKING SUCH REPAIRS SHALL BE TAKEN FROM THE CONTRACTOR'S WARRANTY BOND.
- 8. ALL MATERIALS REQUIRED UNDER THIS SECTION SHALL COMPLY TO NHDOT SPECIFICATIONS; SECTIONS 702, 703 AND 708. GRAVEL - SHALL BE PROVIDED AS SPECIFIED UNDER THE EARTHWORK SPECIFICATIONS AND NOTES. BITUMINOUS CONCRETE — SHALL COMPLY WITH SECTION 401 OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" BY THE STATE OF NEW HAMPSHIRE, DEPARTMENT OF TRANSPORTATION. TYPE (3/4" WINTER BINDER) MIX SHALL BE USED FOR BASE COURSE AND TYPE (1/2") FOR WEARING COURSE.
- PROOF-ROLL PREPARED SUBBASE SURFACE TO CHECK FOR UNSTABLE AREAS AND AREAS REQUIRING ADDITIONAL COMPACTION. NOTIFY ENGINEER OF UNSATISFACTORY CONDITIONS. DO NOT BEGIN PAVING WORK UNTIL DEFICIENT SUBBASE AREAS HAVE BEEN CORRECTED AND ARE READY TO RECEIVE PAVING.
- 10. TACK COAT: APPLY TO CONTACT SURFACES OF PREVIOUSLY CONSTRUCTED ASPHALT OR PORTLAND CEMENT CONCRETE AND SURFACES ABUTTING OR PROJECTING INTO HOT-MIXED ASPHALT PAVEMENT. DISTRIBUTE AT RATE OF 0.04 TO 0.06 GAL. PER SQ. YD. OF SURFACE. ALLOW TO DRY UNTIL AT PROPER CONDITION TO RECEIVE PAVING.
- 11. EXERCISE CARE IN APPLYING BITUMINOUS MATERIALS TO AVOID SMEARING OF ADJOINING CONCRETE SURFACES. REMOVE AND CLEAN DAMAGED
- 12. WHERE NEW PAVEMENT ABUTS EXISTING, A CLEAN STRAIGHT SAWCUT SHALL BE MADE AND A SMOOTH, FLUSH, AND LEVEL TRANSITION OF FINISH SURFACE SHALL BE MAINTAINED. METHODS FOR ON-SITE PAVEMENTS SHALL CONFORM TO APPLICABLE PROVISIONS BELOW:
- ALL TRENCHING AND BACKFILLING SHALL BE COMPLETED AND ANY RESULTANT SETTLEMENT FILLED AND COMPACTED BEFORE THE WORK OF THIS SECTION HAS BEGUN. ROUGH GRADING SHALL HAVE BEEN COMPLETED AND ACCEPTED. ALL UNEVEN AREAS SHALL BE FRESHLY LEVELED AND FILLED TO BRING ROUGH GRADE TO A TRUE AND EVEN SUBGRADE SURFACE BEFORE INSTALLING THE GRAVEL BASE COURSE. BEFORE GRAVEL BASE IS SPREAD, THE SUBGRADE SHALL BE ROLLED BY A SELF-PROPELLED ROLLER WEIGHING NOT LESS THAN 10 TONS. ALL DEPRESSIONS OCCURRING SHALL BE FILLED WITH SUITABLE MATERIALS AND AGAIN ROLLED UNTIL THE SURFACE IS SMOOTH AND HARD. ANY PORTION OF THE SUBGRADE WHICH IS NOT ACCESSIBLE TO A ROLLER SHALL BE REPAIRED THOROUGHLY WITH HAND TAMPERS.
- EXISTING GRAVEL ROADWAY MATERIAL SHALL BE RESHAPED TO THE PROPOSED LINE AND GRADE PRIOR TO PLACING THE CRUSHED GRAVEL BASE.
- GRAVEL BASE SHALL BE SPREAD AND COMPACTED BY ROLLING. BASE SHALL BE HELD BELOW FINISH GRADE BY THICKNESS OF PAVEMENT.
- FOLLOWING THE FINAL ROLLING, THE SURFACE OF THE BASE SHALL BE TESTED WITH A 10' STRAIGHTENED EDGE OR STRINGS FOR DEPRESSIONS OR IRREGULARITIES, WHICH SHALL BE SATISFACTORILY ELIMINATED BY ROLLING OR BY REPLACING THE SURFACE SO THAT THE SURFACE SHALL BE UNIFORM AND TRUE TO THE SPECIFIED GRADE AND CROSS SECTION.
- BITUMINOUS CONCRETE PAVING FOR ROADWAYS AND DRIVEWAY APRONS SHALL BE LAID IN TWO COURSES. THE BASE COURSE CONTAINING THE COARSE AGGREGATE AND THE WEARING COURSE CONTAINING THE FINE AGGREGATE SHALL BE AS INDICATED FOR THE VARIOUS NOTED AREAS OF CONSTRUCTION.
- THE NEWLY CONSTRUCTED SWALES. ALL OTHER STRUCTURES WITHIN AREAS TO BE PAVED SHALL BE SET TO FINAL GRADE AS PART OF THIS

14. SET FRAMES FOR CATCH BASINS CONSISTENT WITH THE ELEVATION OF

- 15. PROTECTION PROTECT ALL PAVEMENT FROM TRAFFIC DURING ALL OPERATIONS AND UNTIL IT IS THOROUGHLY SET AND CURED AND IS NOT DAMAGED BY NORMAL FOOT OR WHEELED TRAFFIC.
- 16. SWEEP AND CLEAN SURFACE TO ELIMINATE LOOSE MATERIAL AND DUST PRIOR TO PLACING PAVEMENT MARKINGS.

PLACEMENT HAVE BEEN VERIFIED WITH ENGINEER.

17. DO NOT APPLY TRAFFIC AND LANE MARKING PAINT UNTIL LAYOUT AND

RESTORATION NOTES

TOPSOILING

TOPSOIL IS REQUIRED ON ANY AREA WHERE IT IS NECESSARY TO ESTABLISH A VEGETATIVE COVER. IF THERE IS INSUFFICIENT NATURALLY OCCURRING TOPSOIL, ADDITIONAL TOPSOIL SHALL BE PROVIDED TO A MINIMUM FINISHED DEPTH OF FOUR INCHES (4").

- 1. LOAM, SANDY LOAM, OR SILTY LOAM SHALL BE USED FOR TOPSOIL
- 2. THE MATERIAL SHALL BE FRIABLE AND FREE OF TREE ROOTS, WEEDS, STONES (GREATER THAN TWO INCHES) AND ANY OTHER DEBRIS. SOIL WHICH HÀS BEEN TREATED WITH HERBICIDE IS UNACCEPTABLE.
- 3. THE MATERIAL SHALL BE TAKEN FROM THE NATURAL SURFACE LAYERS ("A" HORIZON) OF SOILS CAPABLE OF PRODUCING GOOD YIELDS OF
- 4. AREAS WHERE TOPSOIL HAS BEEN REMOVED SHALL BE PROTECTED
- 5. TOPSOIL SHALL NOT BE STOCKPILED WHERE IT WILL INTERFERE WITH ANY DRAINAGE COURSE OR WITHIN TWENTY-FIVE FEET OF A WETLAND
- 6. TOPSOIL SHALL NOT BE COLLECTED OR SPREAD WHILE IT IS WET.
- 7. SUBSURFACES SHALL BE SCARIFIED OR OTHERWISE TILLED TO FACILITATE BONDING OF SOIL LAYERS.

PERMANENT SEEDING

UNLESS OTHERWISE INDICATED OR DIRECTED BY THE ENGINEER, GROUND COVER WILL BE ESTABLISHED BY HYDRO-SEEDING. ALL LOCATIONS ASSOCIATED WITH THIS PROJECT ARE DESIGNATED LAWN AREAS, SLOPE AREAS AND MISCELLANEOUS AREAS WILL BE VERIFIED WITH THE OWNER PRIOR TO

SEED MIXTURE

WHERE USED	MIXTURE	LBS / ACRE
LAWN AREAS	PARK SEED - TYPE 15 RED FESCUE (CREEPING) PERENNIAL RYEGRASS KENTUCKY BLUEGRASS RED TOP	40 50 25 5
	TOTAL LBS PER ACRE	120
SLOPE AND ALL OTHER AREAS OF PROPERTY NOT DESIGNATED AS LAWN	SLOPE SEED — TYPE 44 RED FESCUE (CREEPING) PERENNIAL RYEGRASS BIRDSFOOT TREFOIL RED TOP ALSIKE CLOVER	35 30 5 5 5
	TOTAL LBS PER ACRE	

SITE PREPARATIONS

- 1. SHAPE AND SMOOTH THE SURFACE TO BE SPECIFIED ON DESIGN.
- 2. LIME ACCORDING TO SOIL TEST TO PH 6.5 OR IN THE ABSENCE OF A SOIL TEST, APPLY LIME AT THE RATE OF 2-3 TONS OF GROUND LIMESTONE PER ACRE (100-150 LBS. PER 1000 S.F.)
- 3. FERTILIZE ACCORDING TO SOIL TEST OR AT A RATE OF 1000 LBS. PER ACRE OF 5-10-10 FERTILIZER.
- 4. WORK UP A SEEDBED 3" TO 6" DEEP, THOROUGHLY INCORPORATING THE LIME AND FERTILIZER INTO THE SOIL.
- 5. SEED DURING THE PERIOD FROM AUGUST 10 TO SEPTEMBER 15, OR IN THE SPRING BEFORE MAY 20 FOR PERMANENT COVER. SEEDING AT OTHER TIMES SHALL BE CONSIDERED A TEMPORARY COVER.
- 6. APPLY SEED UNIFORMLY ACCORDING TO DIRECTIONS BY HYDRAULIC
- APPLICATION (HYDROSEEDING).

THE SEEDED AREA IS TO BE PROTECTED FROM EQUIPMENT, TRAMPLING, MULCHING, AND OTHER DESTRUCTIVE ACTIVITY.

TEMPORARY SEEDING

TEMPORARY SEED MIXTURE (NOT FOR WETLAND RESTORATION): WHEN IT IS IMPRACTICAL TO ESTABLISH PERMANENT PROTECTIVE VEGETATION ON DISTURBED EARTH BY OCTOBER 15, USE "CONSERVATION MIX" OR THE

MIN	3 SEED MIXIURE FOR AREAS OUTSIDE OF	CONSTRUCTION	IKAF
	SWITCHGRASS (BLACKWELL OR SHELTER)		4.0
	BIG BLUESTEM (NIAGRA OR KAW)		4.0
	LITTLE BLUESTEM (CAMPER OR BLAZE)		2.0
	SAND LOVEGRASS (NE-27 OR BLAZE)		1.5
	BIRDSFOOT TREFOIL (VIKING)		2.0

TOTAL POUNDS PLS PER ACRE

INOCULUM SPECIFIC TO BIRDSFOOT TREFOIL MUST BE USED WITH THIS MIXTURE. IF SEEDING BY HAND, A STICKING AGENT SUCH AS MILK OR COLA SHALL BE USED TO STICK INOCULUM TO THE SEED. IF SEEDING WITH HYDROSEEDER, USE FOUR (4) TIMES THE RECOMMENDED AMOUNT OF INOCULUM.

MULCH MATERIAL

13.5

AREA OF HIGH FLOWS.

ON SOILS SUBJECT TO EROSION SUCH AS SLOPES AND ON SEEDED AREAS OF TEMPORARY COVER WHERE MOISTURE CONSERVATION IS NECESSARY FOR SEED GERMINATION, MULCH MATERIAL WILL BE USED.

- 1. WHERE FEASIBLE, COMPLETE EARTHWORK AND CONSTRUCTION ACCORDING
- ALL ESSENTIAL GRADING NEEDED TO PREVENT EROSION AND SILTATION IS TO BE COMPLETED PRIOR TO APPLICATION OF MULCH MATERIAL.

MULCH MATERIAL PER 1,000 SF PER ACRE REMARKS

APPLICATION RATE

HAY OR STRAW	70 - 90 LBS	1.5 TONS	LIGHTLY COVER 75% — 90% OF SURFACE, SUBJECT TO BLOWING AND SLIPPING UNLESS ANCHORED.
JUTE OR WOOD			APPLY ACCORDING
EXCELSIOR MATS			TO MANUFACTURER'S

- 3. HYDROMULCH WITH 1 1/2 TONS OF STRAW PER ACRE ON ALL DISTURBED AREAS IMMEDIATELY AFTER SEEDING.
- 4. MULCHES SUSCEPTIBLE TO BLOWING AND THOSE ON STEEP SLOPES WILL REQUIRE ANCHORING.

CONSTRUCTION SEQUENCE

- PERFORM TREE PRUNING AND TRIMMING AS NEEDED. COORDINATE ALL TREE PRUNING AND TRIMMING WITH THE TOWN, ENGINEER, AND PROPERTY OWNERS PRIOR TO CUTTING ANY TREES.
- INSTALLATION OF ALL TEMPORARY SOIL EROSION CONTROL METHODS, INCLUDING BUT NOT LIMITED TO THE FOLLOWING: STABILIZED STONE CONSTRUCTION ENTRANCE, SILT FENCE, HAY BALES, COMPOST SOCK, AND STONE CHECK DAMS.
- INSTALL TEMPORARY WATER SYSTEM TO ALL IMPACTED PROPERTIES. COORDINATE THE INSTALLATION, TESTING, SAMPLING, AND CONNECTION OF THE TEMPORARY WATER SERVICE WITH THE TOWN, ENGINEER, AND PROPERTY OWNERS.
- CONSTRUCT NEW WATER MAIN PIPES, VALVES, FITTINGS, AND HYDRANT. PERFORM ALL TESTING, SAMPLING, FLUSHING OF THE NEW WATER MAINS.
- INSTALL NEW WATER SERVICES TO EACH PROPERTY. FINAL CONNECTION OF THE NEW WATER SERVICE AND WATER METER MUST BE COORDINATED WITH THE TOWN, ENGINEER, AND PROPERTY OWNERS.
- REMOVE THE TEMPORARY WATER SYSTEM ONCE ALL WATER SERVICES ARE CONNECTED.
- PROVIDE FOR AND MAINTAIN EXISTING DRAINAGE FLOW DURING ALL PHASES OF CONSTRUCTION AS NECESSARY. MONITOR AND MAINTAIN ALL EROSION CONTROL MEASURES DURING ALL PHASES OF CONSTRUCTION.
- EXCAVATE TO SUBGRADE ELEVATIONS AS SHOWN ON THE TYPICAL SECTIONS, FINE GRADE ROADWAY TO TOP OF SUBGRADE ELEVATIONS. PROOF ROLL ROADWAY AND INSTALL ROADWAY SUBBASE MATERIALS AS SHOWN ON THE TYPICAL SECTIONS. PERFORM THE APPROPRIATE MATERIAL TESTING AS REQUIRED BY THE TOWN AND ENGINEER.
- REGRADE/CLEAN DITCHLINES AS SHOWN ON THE GENERAL PLANS, PROVIDE FOR AND MAINTAIN EXISTING DRAINAGE FLOW DURING CONSTRUCTION INCLUDING ANY WATER DIVERSIONS AND/OR PUMPING AS NECESSARY. PROVIDE FOR AND MAINTAIN EXISTING UTILITIES INCLUDING ANY TEMPORARY SERVICES AS NECESSARY. MONITOR AND MAINTAIN ALL EROSION CONTROL MEASURES DURING CONSTRUCTION.
- INSTALL CRUSHED STONE COARSE & FINE BASE COURSES AS SHOWN ON
- THE GENERAL PLANS AND TYPICAL SECTIONS. 11. COMPLETE WINTER BINDER COURSE PAVING.
- REMOVE AND REPLACE ALL CB, DMH, AND SMH COVERS WITH NEW NHDOT COVERS. INSTALL THE NEW COVERS TO THE BINDER COURSE FINISHED GRADE. RAISE ALL WATER SHUT OFFS AND WATER GATE VALVES TO FINISHED GRADE.
- 13. INSTALL CRUSHED GRAVEL SHOULDERS.
- 14. CONSTRUCT INDIVIDUAL PAVED APRONS AT DRIVEWAYS.
- 15. COMPLETE TOP COURSE PAVING OF ROADWAY AND DRIVEWAYS.
- 16. INSTALL SIGNS.
- 17. LOAM AND HYDROSEED ANY DISTURBED SURFACES ALONG EDGES OF PAVEMENT AS REQUIRED.
- 18. INSPECT AND MAINTAIN ALL EROSION AND SEDIMENT CONTROL MEASURES UNTIL VEGETATION IS ESTABLISHED.



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Notes

Appd YYYY.MM.DD Issued ______ AJG AJG JRB March 2023

Dwn. Dsgn. Chkd. YYYY.MM.DD File Name: 50165 NOTES

Client/Project Logo

Permit/Seal

Client/Project TOWN OF WOLFEBORO

GREEN ST ROADWAY RECONSTRUCTION PROJECT

Wolfeboro, New Hampshire

CONSTRUCTION NOTES

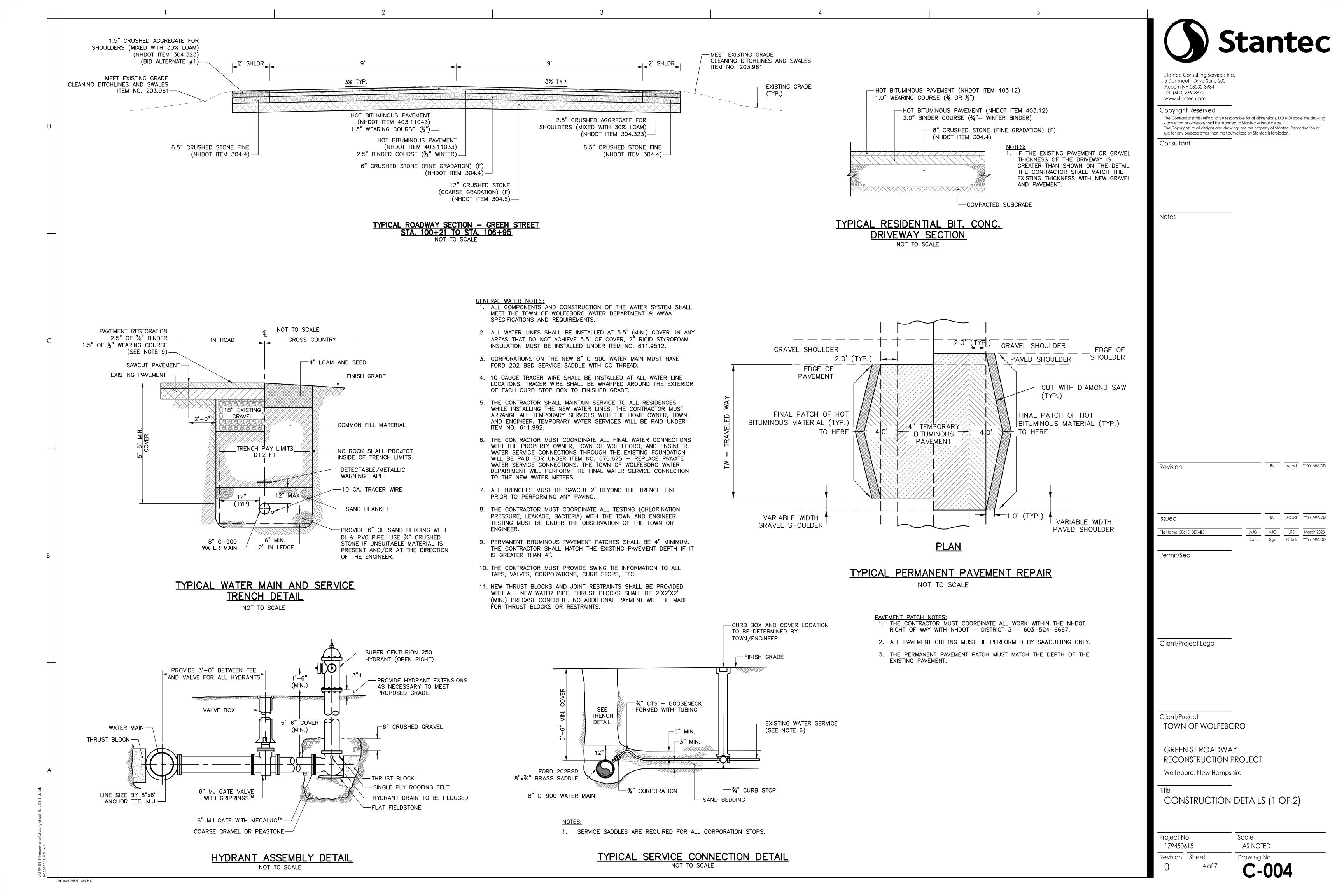
3 of 7

Project No. 179450615 Revision Sheet

AS NOTED Drawing No.

Scale

ORIGINAL SHEET - ARCH D



COMPOST FILLED SILT SOCK

(12"-18" TYP.)

FLOW

WORK AREA

TOP OF GROUND

COMPOST FILLED SILT SOCK

(12"-18" TYP.)

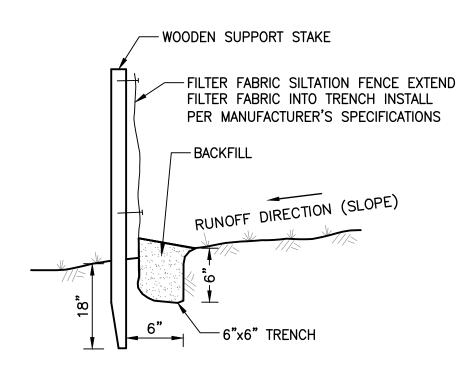
3"-4"

PROTECTED

AREA

SILT SOCK DETAIL

NOT TO SCALE



SILT FENCE DETAIL

NOT TO SCALE

NOTES

- 1. SILT SOCK SHALL BE FILTREXX™ SILTSOXX™ OR APPROVED EQUIVALENT.
- 2. SEE SPECIFICATIONS FOR SOCK SIZE AND COMPOST FILL REQUIREMENTS.
- 3. SILT SOCK SHALL BE INSPECTED PERIODICALLY AND AFTER ALL STORM EVENTS, AND REPAIR OR REPLACEMENT SHALL BE PERFORMED AS NEEDED.
- 4. AFTER IMPACT AREA HAS BEEN FULLY STABILIZED, THE COMPOST MATERIAL SHALL BE DISPERSED ON SITE, AS DETERMINED BY THE ENGINEER.

FILTER BASKET NOTES:

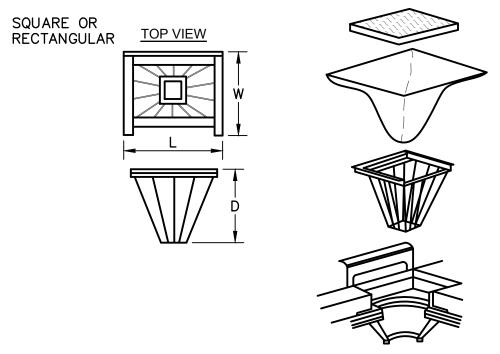
- 1. INLET BASKETS SHALL BE USED ON ALL CATCH BASINS IN PAVEMENT AREAS WITHIN THE PROJECT LIMITS AND/OR AS DIRECTED BY THE ENGINEER. INLET FILTER BASKETS SHALL BE "METAL—ERA" OR APPROVED EQUAL.
- 2. FILTER FABRIC SHALL BE PUSHED DOWN AND FORMED TO THE SHAPE OF THE BASKET. THE SHEET OF FABRIC SHALL BE LARGE ENOUGH TO BE SUPPORTED BY THE BASKET FRAME WHEN HOLDING SEDIMENT AND EXTEND AT LEAST 6 INCHES PAST THE FRAME. THE INLET GRATE SHALL BE PLACED OVER THE BASKET/FRAME AND WILL SERVE AS THE FABRIC ANCHOR.
- 3. THE FILTER FABRIC SHALL BE A GEO—TEXTILE FABRIC: POLYESTER, POLYPROPYLENE, STABILIZED NYLON, POLYETHYLENE OR POLYVINYLIDENE CHLORIDE MEETING THE FOLLOWING SPECIFICATIONS:

GRAB STRENGTH: 45 Ib. MINIMUM IN ANY PRINCIPAL DIRECTION (ASTM D1682).
MULLEN BURST STRENGTH: MINIMUM 60 psi (ASTM D774).

THE FABRIC SHALL HAVE AN OPENING NO GREATER THAN A NUMBER 20 U.S. STANDARD SIEVE AND MINIMUM PERMEABILITY OF 120 gpm/sq. ft.

THE INLET BASKET SHALL BE INSPECTED WITHIN 24 HOURS AFTER EACH RAINFALL OR DAILY DURING EXTENDED PERIODS OF PRECIPITATION. REPAIRS SHALL BE MADE IMMEDIATELY, AS NECESSARY, TO PREVENT PARTICLES FROM ENTERING THE DRAINAGE PIPING SYSTEM AND/OR CAUSING SURFACE FLOODING.

INLET BASKETS SHALL BE MAINTAINED IN PLACE UNTIL ALL PAVING IS COMPLETED AND ALL UNPAVED AREAS HAVE BEEN STABILIZED WITH VEGETATION.



INLET FILTER BASKET
NOT TO SCALE

TRAFFIC SIGN SUMMARY

IDENTIFI— CATION	SIZE OF SIGN		TEXT	TEXT DIMENSIONS		NUMBER OF	COLOR		POST SIZE AND	UNIT AREA IN	AREA IN SQUARE		
NUMBER	WIDTH	HEIGHT	- '-'	LETTER HEIGHT	VERTICAL SPACING	ARROW	SIGNS REQUIRED	BACK- GROUND	LEGEND	BORDER	NUMBER REQUIRED	SQUARE FEET	FEET
R1-1	30	30	STOP		2009 TCD	_	1		2009 FCD		U-CHNL 1	6.25	6.25
R8-3a	24	30	NO PARKING		SEE		2		SEE		U-CHNL 2	5.00	10.00
_	24	30	NOT A THRU STREET	4D	2.5		1	WHITE	BLACK	BLACK	U-CHNL 1	5.00	5.00

Stanted Stanted

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Notes

Revision

 Issued
 By
 Appd
 YYYY.MM.DD

 File Name: 50615_DETAILS
 AJG
 AJG
 JRB
 March 2023

 Dwn.
 Dsgn.
 Chkd.
 YYYY.MM.DD

Permit/Seal

Client/Project Logo

Client/Project
TOWN OF WOLFEBORO

GREEN ST ROADWAY
RECONSTRUCTION PROJECT

Wolfeboro, New Hampshire

_____ Title

CONSTRUCTION DETAILS (2 OF 2)

Project No. 179450615 Revision Sheet

AS NOTED

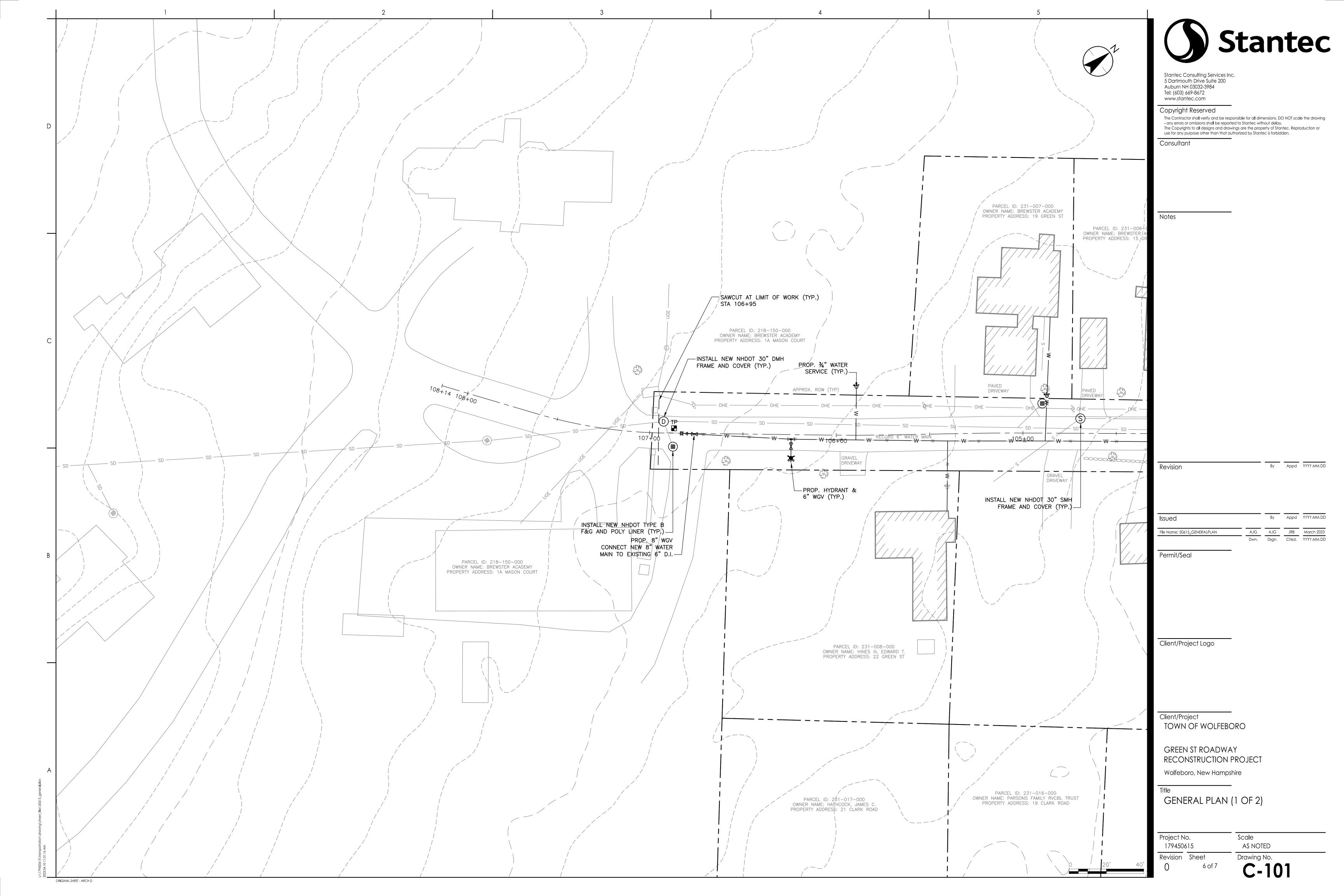
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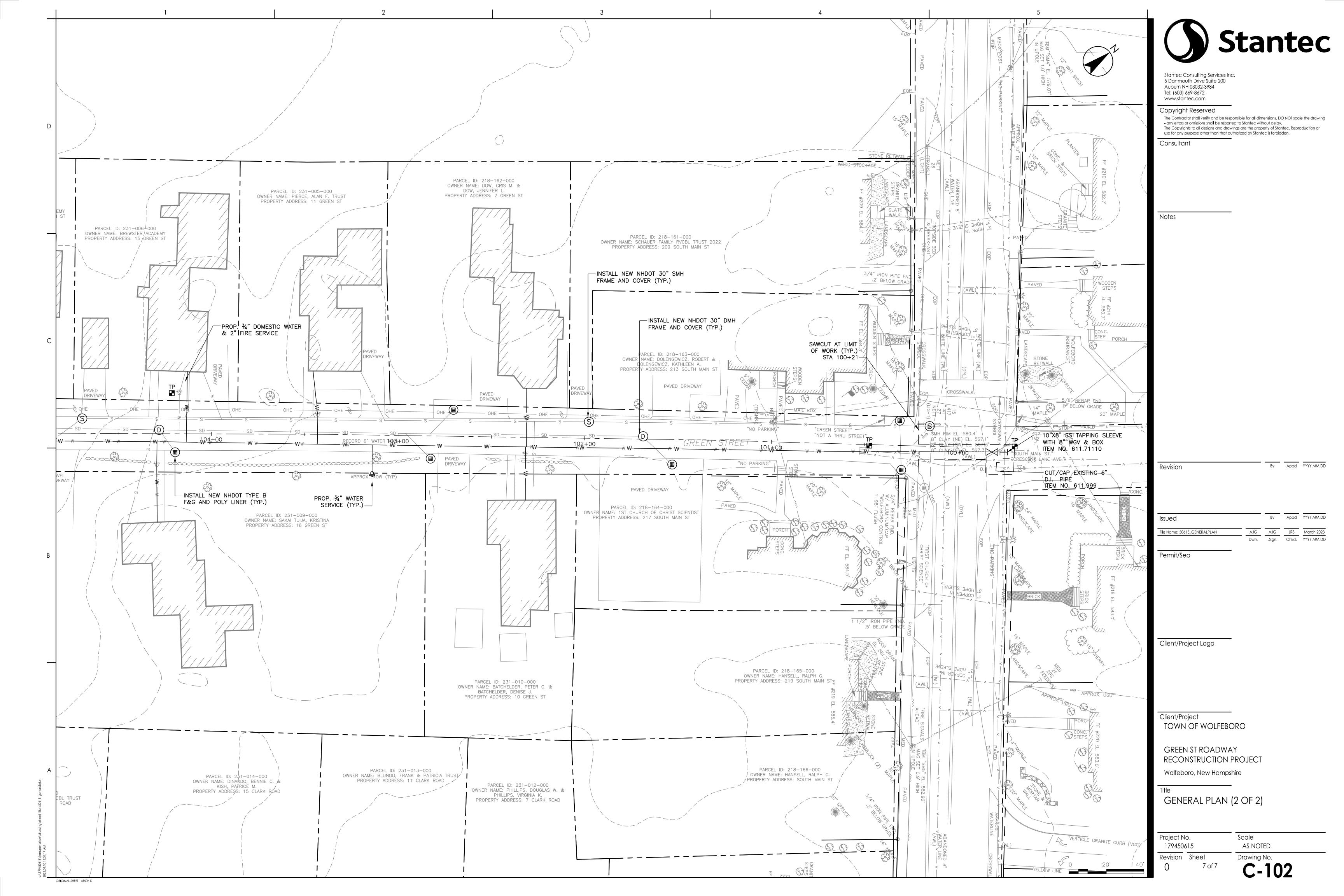
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ORIGINAL SHEET - ARCH D





NOTE: Revised Standards TC-1 through TC-8 amend Part VI of the 2009 Edition of the MUTCD by superseding or supplementing certain Sections. They shall be used in conjunction with the MUTCD and the Specifications for work zone traffic control on all projects.

- 1. Section 6C.04, Table 6C-1 and Section 6H-01, Table 6H-3. "Urban (low speed)" shall be defined as those roadways with regulatory speed limits of 30 mph or less; "Urban (high speed)" shall be defined as those roadways with regulatory speed limits
- 2. Section 6F.03, Sign Placement. Add the following paragraph as a **"Standard"** heading:
 - O1a Actual placement of temporary traffic control signs shall be carefully considered to avoid obstructing existing signs or allowing existing signs, vegetation or other physical features to obstruct or limit visibility to temporary traffic control signs.

 Temporary traffic control signs shall also be placed at locations that avoid overwhelming motorists with information when combined with existing signs.
- 3. Section 6F.17 Positioning of Advanced Warning Signs. Add the following sentence as "Guidance" and "Option", respectively after sentence 07:

Guidance:

- 08 When multiple operations are occurring in the same area, duplication of the advance warning signs, e.g. ROAD WORK AHEAD, ROAD WORK ½ MILE, etc., should be avoided.

 Option:
- 1n cases where room for advance warning signage is severely limited, some of the general advanced warning signs (e.g. ROAD WORK AHEAD) may be eliminated in order to provide adequate space for driver to see and comprehend the warning signs requiring driver action, e.g. LANE ENDS MERGE LEFT, FLAGGER AHEAD, etc.
- 1. Section 6F.64, Cones. Add the following to the **"Standard"**:
 - 01a Cones shall not be used at night as the primary channelization device, except during work hours.
- 5. Section 6F.65, Tubular Markers. Replace paragraphs 01 and 02 of the "Standard" section with the following:
 - 1 Tubular markers shall be predominately orange and shall not be less than 42 inches high and 3 inches wide when facing road users. They shall be made of material that can be struck without causing damage to the impacting vehicle. Refer to MUTCD 6F-65 Paragrah 3 for delineation color and type.
- 6. Section 6F.67, Drums. Add the following sentences after Sentence number 01:

Standard:

01a Drums shall be the primary delineation device on divided highways for all tapers and tangents.

Option:

- O1b Cones or tubular markers may be used, only in the tangent sections of the lane closure, when inadequate width, geometric constraints or the duration of the operation (short-duration or mobile, see 6G.02 for Work Duration definitions) necessitates the use of a narrower or more easily moved channelizing device.
- 7. Section 6F.78 Temporary Markings. Add to the "Standard" the follow sentences:
 - 05a All temporary markings on divided highways shall be 4-inch removable tape or paint conforming to MUTCD Chapter 3, Section 3A.
 - 05b All temporary markings shall be offset 1-foot from the final striping location.
 - 05c All temporary white broken-line pavement markings for traffic moving in the same direction shall be retroreflectorized paint or tape. Temporary paint or tape markings shall have a cycle length of 40 feet long with minimum 4-foot long skip and 36-foot long gap. Temporary tape shall be removed prior to any overlays and after permanent pavement markings have been applied.
 - 05d Stop lines shall be installed during temporary conditions and shall be retroreflectorized paint or tape.

Replace "Guidance" paragraph 03 with:

- Edge lines, channelizing lines, lane reduction transitions, gore markings, and non-longitudinal lines (e.g., railroad crossings, crosswalks, words, symbols, etc.) are usually not required for temporary situations. Their use should be evaluated on a project by project basis based on field conditions, relative traffic speeds and volumes, and the use of other traffic control devices. When used, temporary markings for these types of longitudinal and non-longitudinal lines shall be retroreflectorized paint or tape and conform to MUTCD Part 3 Chapters 3A and 3B.
- 8. Section 6F.85 Temporary Traffic Barriers. Add the following to the "Standard" paragraph 06:
 - Temporary end treatments in the form of sand barrels and water filled arrays shall not be used from November 1st through April 15th unless they are greater than ten feet from the travelway (measured to the face) or specifically approved in writing by the Engineer. If approved by the Engineer for winter use, the sand or water shall be treated in accordance with the manufacturer's recommendations to prevent freezing.
 - 06b Impact attenuators shall be marked with a Type 3 Object Marker per Section 2C.63 Object Marker Design and Placement Height paragraph 02.
- 9 Section 6G.05 Work Affecting Pedestrian and Bicycle Facilities. Add the following to the "Support" paragraph 01:
 - 01a R4-11 (Bike May Use Full Lane) sign should be used when the clear width of a single lane and shoulder is less than 14', except when the existing lane and shoulder in the general vicinity of the work provides less than 14' clear. This sign is optional where operational controls are used, and during mobile, short duration, and short term stationary work durations as defined by Section 6G.02. This sign shall not be used when the speed limit is over 40 mph.
- 10 Section 6H.01, Typical Applications. Add the following paragraph to the Option heading:
 - Many diagrams show ROAD WORK (W20-1), ROAD WORK NEXT XX MILES (G20-1), and END ROAD WORK (G20-2a) signs being used for the activities. These signs may be omitted if the activity is being performed within the limits of a larger project and the Advance Warning and/or Termination Signs for the larger project provide reasonable warning to the motorist for the activity.
- Section 6H.01, Figure 6H-14. The diagram for the unsignalized crossing of a Haul Road shows interim tape and a NO PASSING ZONE (W-14-3) sign to deter passing maneuvers. In lieu of interim tape, cones may be placed along the centerline, using a maximum spacing of 40-feet.
 - In both diagrams, add a TRUCK CROSSING (W8-6) sign at a distance "B" in advance of the DO NOT PASS (R4-1) sign. Show the ROAD WORK AHEAD (W20-1a) sign at a distance "C" in advance of the TRUCK CROSSING sign. (See Table 6H-3 for distance
- 12 Section 6H.01, Figure 6H-36. Make the following revisions:
 - a. Use REVERSE CURVE (W1-4 series) signs which show side-by-side arrows, one arrow for each open lane, at each location that the sign is shown.

WORK ZONE TRAFFIC CONTROL

AMENDMENTS TO PART VI MUTCD (2009) NO. TC-1

REVISION DATE

08/03/2004

03/16/2017

11/28/2018

05/17/2019

STANDARD

*.DGN FILE NAME TC-1

STANDARD PLANS



STANDARD NO. TC-1 Examples of traffic control operations where **Uniformed Officers and flaggers are typically not** needed:

- 1. Shoulder work.
- 2. Work behind barrier.

Examples of traffic control operations where flaggers should be used include:

- 1. Alternating 1-way traffic (stop/slow paddles must be used).
- 2. Directing traffic through low volume intersections.
- 3. Assisting trucks and equipment in and out of work areas.
- 4. Providing coverage at side roads and driveways during mobile operations (e.g. paving, striping, etc.).
- 5. Directing pedestrians and bicyclists through the work zone.

Examples of traffic control operations where Uniformed Officers may be used include:

- 1. Directing traffic through complex intersections, especially where signals are being overridden.
- 2. Assisting construction vehicles and equipment in and out of work areas on high speed(> 45 mph), high volume facilities(> 15000 vpd). Note: If an access area is anticipated to be in place for an extended period of time and it is determined that assistance is required for the safe exit and entry of construction vehicles, then a cost analysis should be completed to determine if stationary measures (e.g. signals) would be more cost effective than officers or flaggers.
- 3. Rolling roadblock operations on interstate and turnpike facilities and other multi-lane L.A.R.O.W. highways.
- 4. If a uniformed officer is already on site for other needs (enforcement or presence), then the officer may be asked to supplement these duties by providing limited duration traffic control that would otherwise be covered by a flagger. However, the officer must be adequately trained for the flagger operation to be performed and must use appropriate equipment and techniques (which may include the use of stop/slow paddles).
- 5. If approved, officers may be hired as a speed deterrent and/or to increase driver awareness through a work zone under the following conditions:
 - a. The work zone has a posted speed of 45 mph or higher and an average daily traffic (ADT) volume of 15,000 vpd or greater; and
 - b. The work zone presents a unique safety issue, such as a high rate of crashes, vehicles traveling at excessive speeds, poor highway geometrics, excessive East-West sun glare; workers exposed to traffic; and/or construction equipment frequently entering and exiting the work zone.
- 6. In rare cases, a presence officer may be approved for use on low speed (< 45 mph) or low volume (< 15,000 vpd) roads if a unique safety issue exists and other speed deterrent or driver awareness measures are proven ineffective.
- 7. The use of law enforcement may be considered for nighttime operations. When used at night the use of the blue lights and positioning should be carefully considered. Excessive use of police vehicles with lights at night, or inappropriate positioning of these vehicles may actually detract from the positive guidance the work zone traffic control devices provide. When used for nighttime work, blue lights should be dimmed and headlights should be off.

See complete Flagger and Uniformed Officer guidelines at this link:

http://www.nh.gov/dot/org/projectdevelopment/construction/documents/FlaggerPoliceUseGuidelines.pdf

UNIFORMED OFFICER PLACEMENT IN THE WORK ZONE

If Uniformed Officer with Vehicle use has been approved for presence, cruiser placement is recommended as follows:

- 1. Park in the shoulder or median, not in the travel lane.
- 2. Do not park behind the Truck Mounted Attenuator (TMA).
- 3. Do not park in the buffer zone. If buffer zone presence is needed, then consideration should be given to installing a truck TMA instead.
- 4. Do not park in the taper.
- 5. Locate the police cruiser between the 1st and 2nd signs (from the taper).
 - a. Urban (Low </= 30 mph) 150' from the taper.
 - b. Urban (High >/= 35 mph) 525' from the taper.
 - c. Rural = 750' from the taper.
 - d. Expressway/Freeway = 1750' from the taper.
- 6. Consider having the cruiser face traffic for stationary operations.
 - a. Recommended cruiser positioning for moving operations:
 - I. Less than 5 mph face traffic (e.g. crack seal).
 - II. Greater than 5 mph face work (e.g. striping, rumble strips).
- 7. Stay ¼ mile in front of queue.
- 8. If a second Officer is used for enforcement, and there is no queue, the enforcement officer should be immediately after the work zone. If there is a traffic queue then the enforcement officer should be several miles before the backup queue and presence Officer.
- 9. Hands free and cell phone use should be only for work zone activity.
- 10. Headlights off, dim blue lights at night if possible.

WORK ZONE TRAFFIC CONTROL

UNIFORMED OFFICERS
AND FLAGGERS

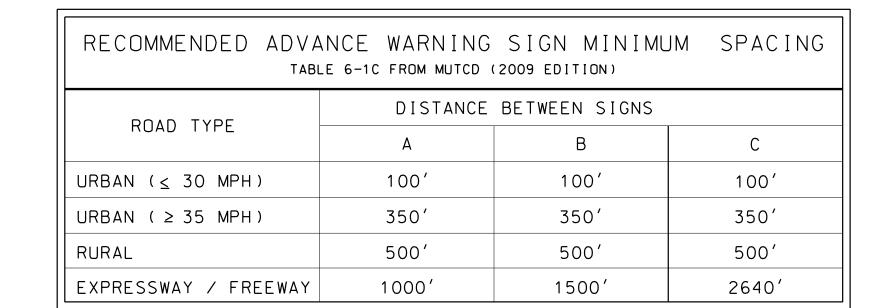
STANDARD NO. TC-2 REVISION DATE 03/16/2017

*.DGN FILE NAME TC-2

STANDARD PLANS



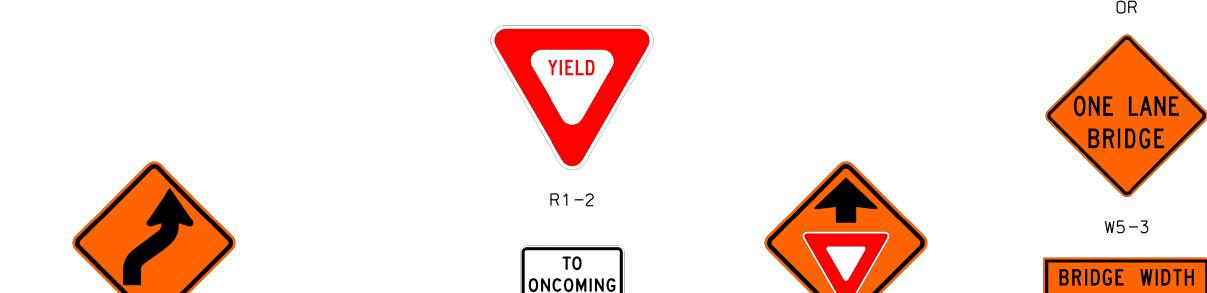
STANDARD NO. TC-2

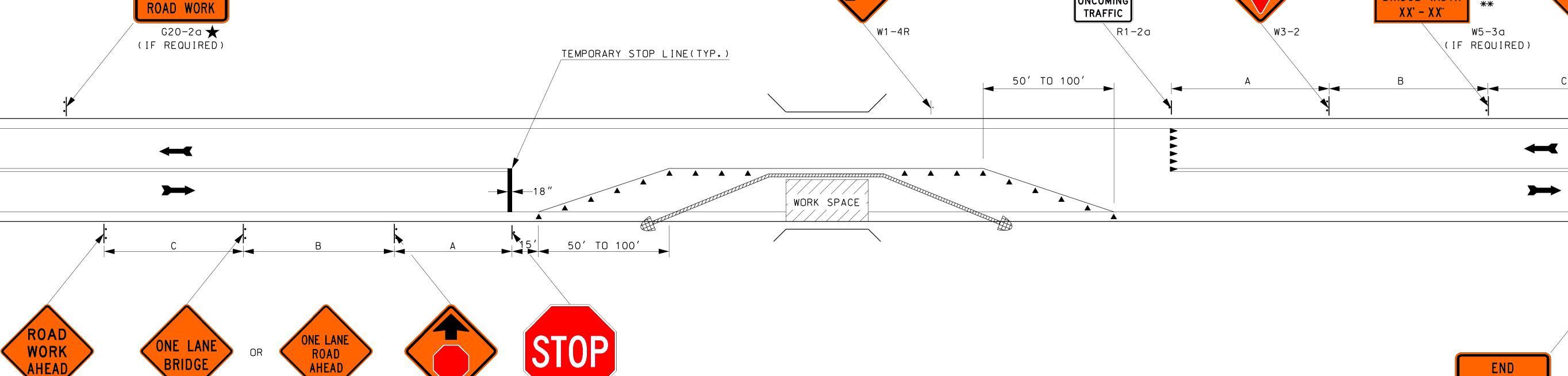


END

TYPICAL APPLICATION LANE CLOSURE: TWO-LANE ROAD WITH LOW TRAFFIC VOLUMES







GENERAL NOTES

W20-1a ★

- ★ SEE AMENDMENT NO. 10 ON TC-1
- ** POSTED BRIDGE WIDTH SHALL BE 1 FOOT LESS THAN ACTUAL WIDTH.

W5 - 3

BRIDGE WIDTH

X X' - X X''

W5-3a (IF REQUIRED)

1. THIS TYPICAL APPLICATION SHOULD BE USED AS AN ALTERNATE TO MUTCD FIGURE TA-11 WHEN CONSTRUCTION ACTIVITIES ARE FOR A PERIOD LONGER THAN THAT CONSIDERED INTERMEDIATE-TERM WORK (SEE MUTCD SECTION 6G.02).

W20-4

W3 - 1

R1-1

- 2. EXISTING PAVEMENT MARKINGS SHALL BE REMOVED AND TEMPORARY MARKINGS INSTALLED AS ORDERED.
- 3. REGULATORY SIGN PLACEMENT SHALL BE APPROVED BY BUREAU OF TRAFFIC.
- 4. HAZARD IDENTIFICATION BEACONS OR TYPE A FLASHING WARNING LIGHTS MAY BE MOUNTED WITH WARNING SIGNS, IF WARRANTED.
- 5. THE USE OF BARRIER IS ANTICIPATED FOR MOST SITUATIONS REQUIRING APPLICATION OF THIS LANE CLOSURE. REQUIRED BARRIER PROTECTION SHALL BE DETERMINED AS DESCRIBED IN THE MOST CURRENT EDITION OF THE ROADSIDE DESIGN GUIDE AS ADOPTED BY THE DEPARTMENT TAPER RATES FOR BARRIER ARE FOUND ON THE PORTABLE CONCRETE BARRIER STANDARD (GR-23).
- 6. CRASH CUSHIONS SHALL BE DELINEATED WITH TYPE 3 OBJECT MARKERS. SEE MUTCD FIGURE 2C-13.

STATE OF NEW HAMPSHIRE

SPECIAL DETAILS

DEPARTMENT OF TRANSPORTATION . BUREAU OF HIGHWAY DESIGN

YIELD LINES

LEGEND

PORTABLE BARRIER

IMPACT ATTENUATOR

CHANNELIZING DEVICES

WORK

(W20-1a ★

LANE CLOSURE: TWO-LANE ROAD

WITH LOW TRAFFIC VOLUMES

REVISION DATE DGN STATE PROJECT NO. SHEET NO. TOTAL SHEETS

03-16-17 + WINTH-lowvol _ 3 12

ROAD WORK

(IF REQUIRED)

 \triangledown

G20-2a ★

NOT TO SCALE